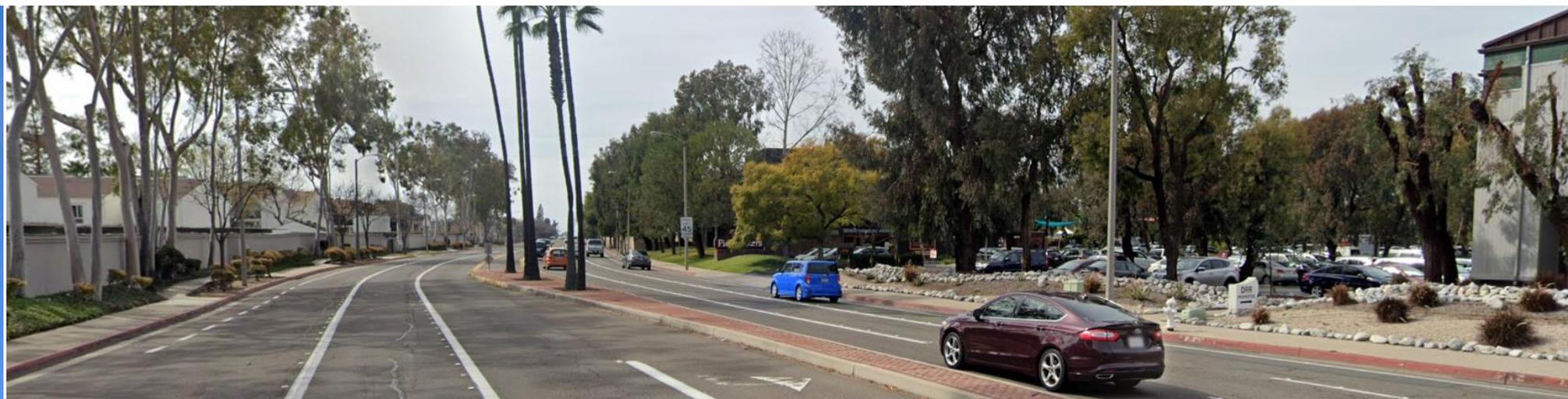




# Adams Avenue Improvement Project Conceptual Design Alternatives

**Bikeway & Walkability Committee Meeting**

**Costa Mesa City Hall – Conference Room 1A – 2/5/2020**



# Adams Avenue – Project Limits



# Project Background

- **Adams Avenue** – City boundary to Royal Palm Drive (1.3 mi.)
  - 4 intermediate signalized intersections
  - 3 lanes per direction, 5' bike lanes, 10' striped median; intermittent sidewalks
  - 36,000 – 39,000 ADT Volumes
- **Existing Challenges/Deficiencies**
  - Non-compliant ADA curb ramps; Cracked and deteriorated pavement; Missing sidewalk; Utility obstructions in sidewalk (equipment, overhead SCE lines, tree wells)



# Project Background, cont'd.

- **Project evolved over time based on**
  - **Community Input**
  - **Need for sidewalks**
  - **Deteriorated Pavement**
  - **Bicycle Facility Upgrades**
- **Project included in:**
  - **Active Transportation Plan**
  - **East-West Multi-Purpose Trails Plan**
  - **OCTA District 2 Bicycle Plan**



# Project Objectives

- **Protected Bikeways**
- **Pavement Rehabilitation**
- **Sidewalk/Accessibility Improvements**
- **Traffic Signal Modifications**
- **Median/Landscape Improvements**
- **Utility Undergrounding**

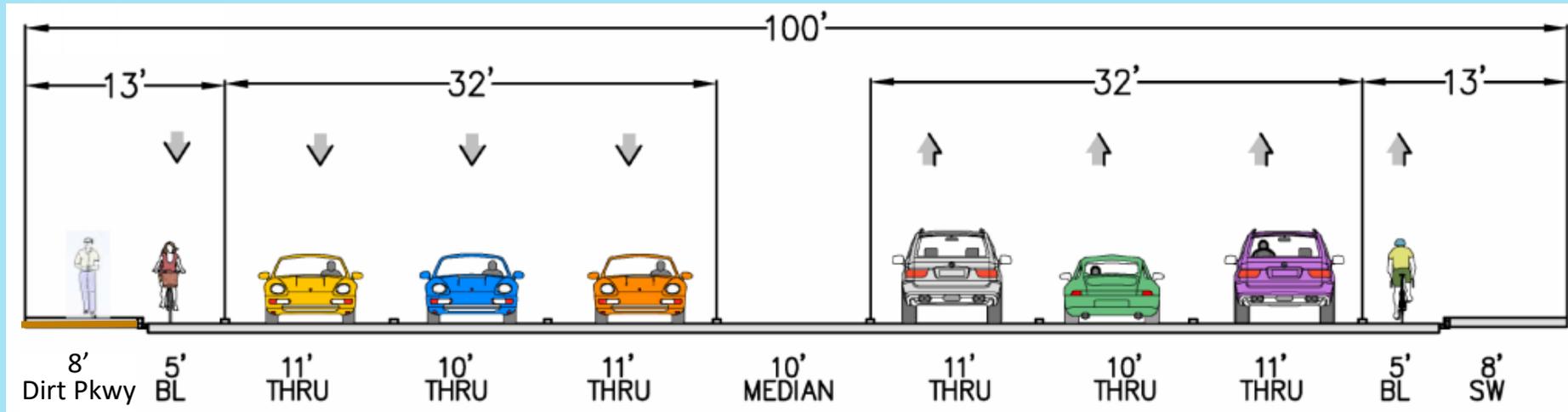


# Existing Roadway Configuration



# Existing Roadway – Typical Cross Section

*At  
Mid-  
Segment*



# Design Concept Alternative #1 (“ALT1”)

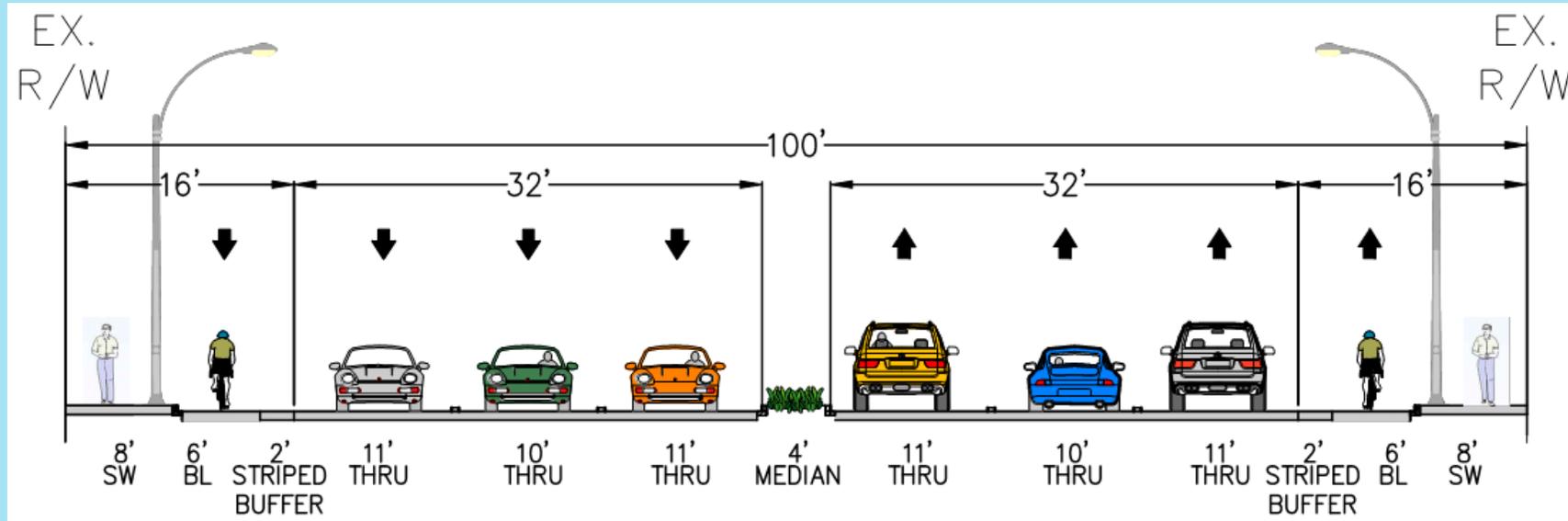
## Buffered Bike Lanes

- Center median area reduced by 6' (10' to 4')
- **Add raised center median & landscaping**
- Remaining 6' balance re-distributed to each side (3' per dir.)
- **2' striped buffer zone added between travel lanes & bike lane**
- **Bike lane widened by 1 ft (5' to 6')**
- At Intersections:
  - » Maintain existing 10' wide LT lanes
  - » Sidewalk width adjusted by 1' (8' to 7'), and re-distribute to bike lane (+1'), remove 2' striped buffer

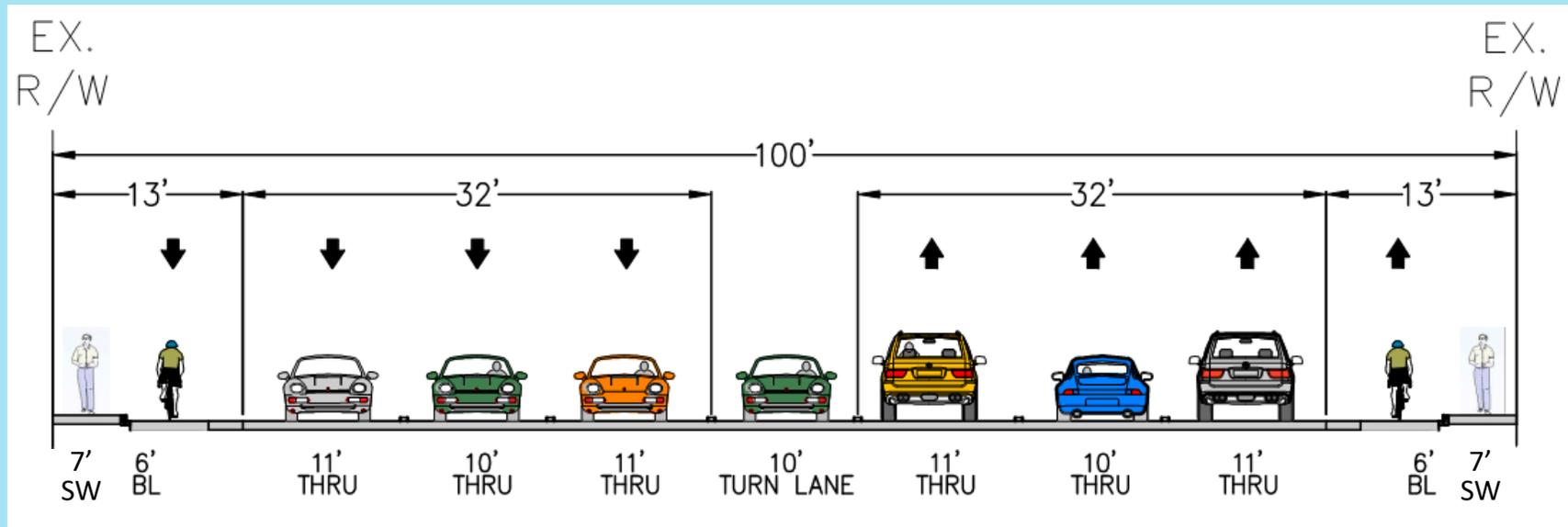


# ALT1 – Typical Cross Section

*At  
Mid-  
Segment*



*At Intersections  
With  
Left-Turn Pocket*



# ALT1 – Street View Rendering



# Design Concept Alternative #2 (“ALT2”)

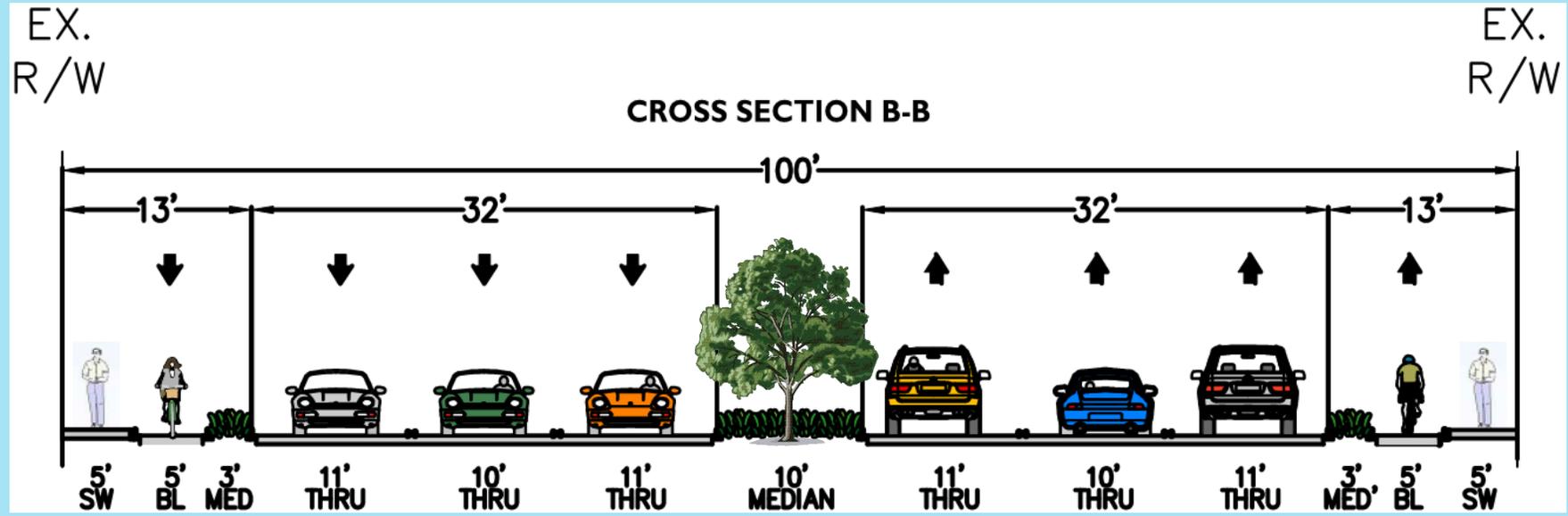
## Cycle Track

- Adjusted sidewalk width by 3’ on both sides (8’ to 5’)
- **Construct 10’ wide, raised center median, landscaping & trees**
- **Add 3’ wide raised landscaped parkway to separate travel lanes from cycle tracks**
- At Intersections: Maintain existing 10’ wide LT lanes

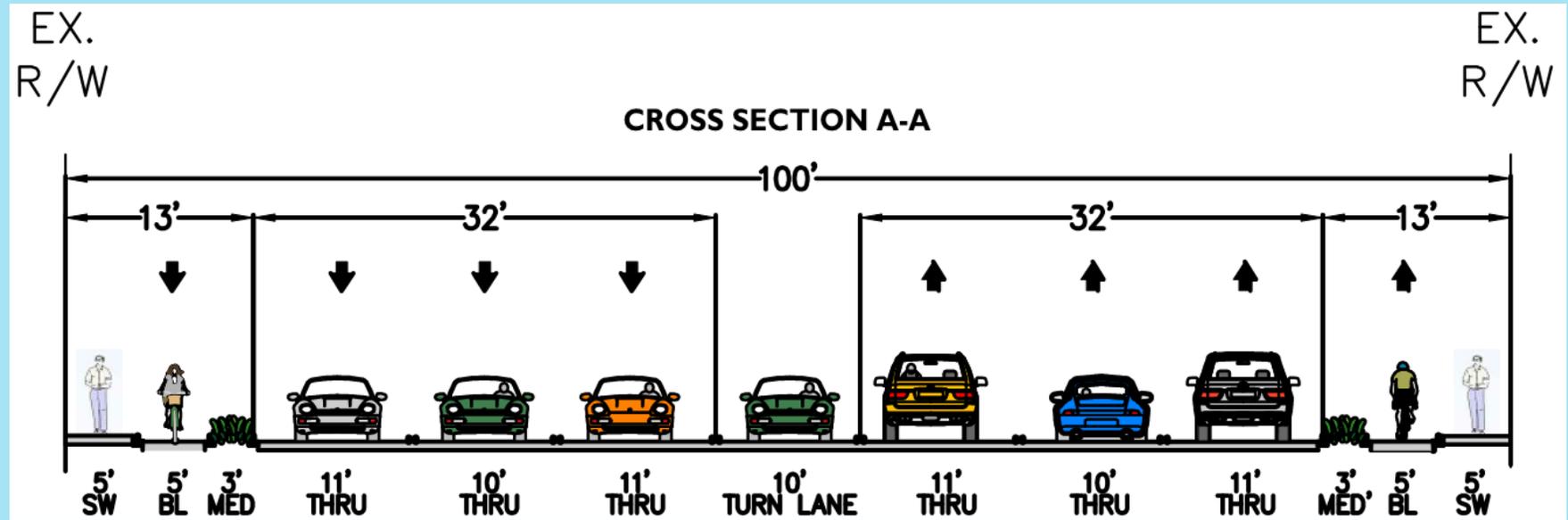


# ALT2 – Typical Cross Section

*At Mid-Segment*



*At Intersections With Left-Turn Pocket*



# ALT2 – Street View Rendering



# Design Concept Alternative #3 (“ALT3”)

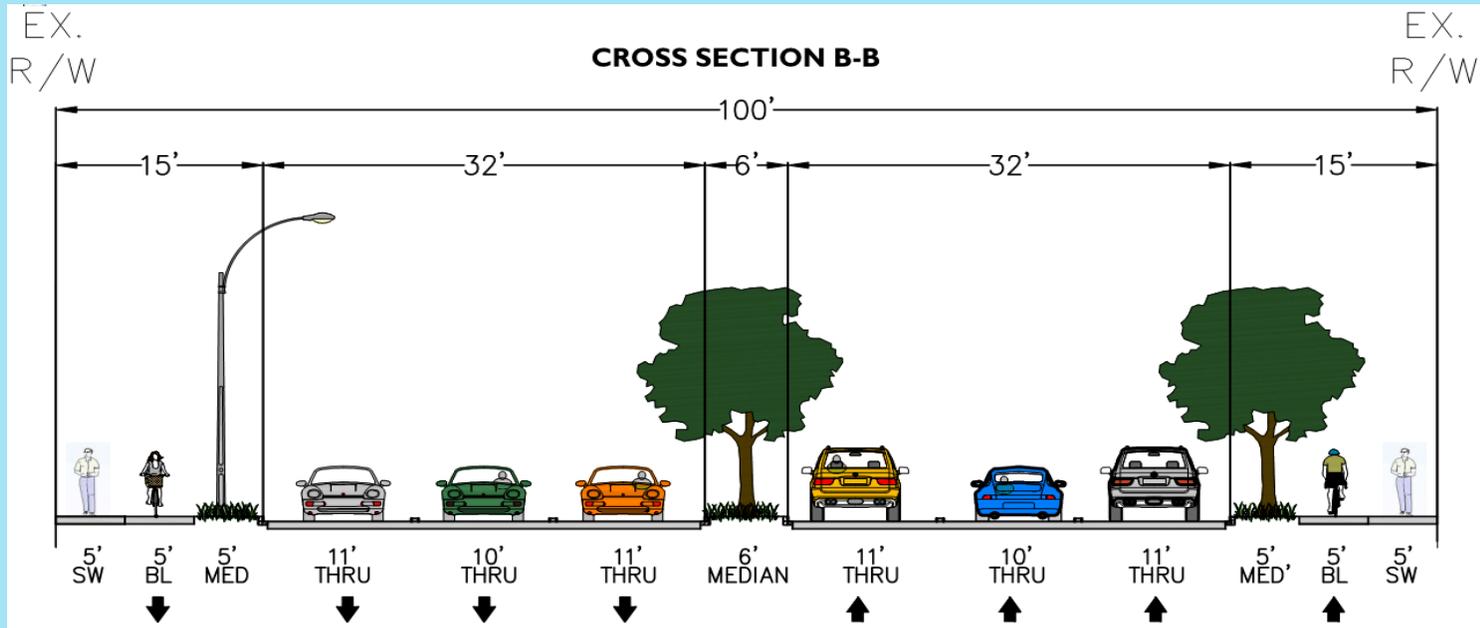
## Shared-Use Path

- Center median area reduced by 4' (10' to 6')
- **Add raised center median & landscaping**
- **Widen existing sidewalk area to provide:**
  - » **10' Shared-Use Path at sidewalk grade**
  - » **5' Landscaped Parkway separating travel lanes from shared-use path**
- At Intersections: Maintain existing 10' wide LT lanes

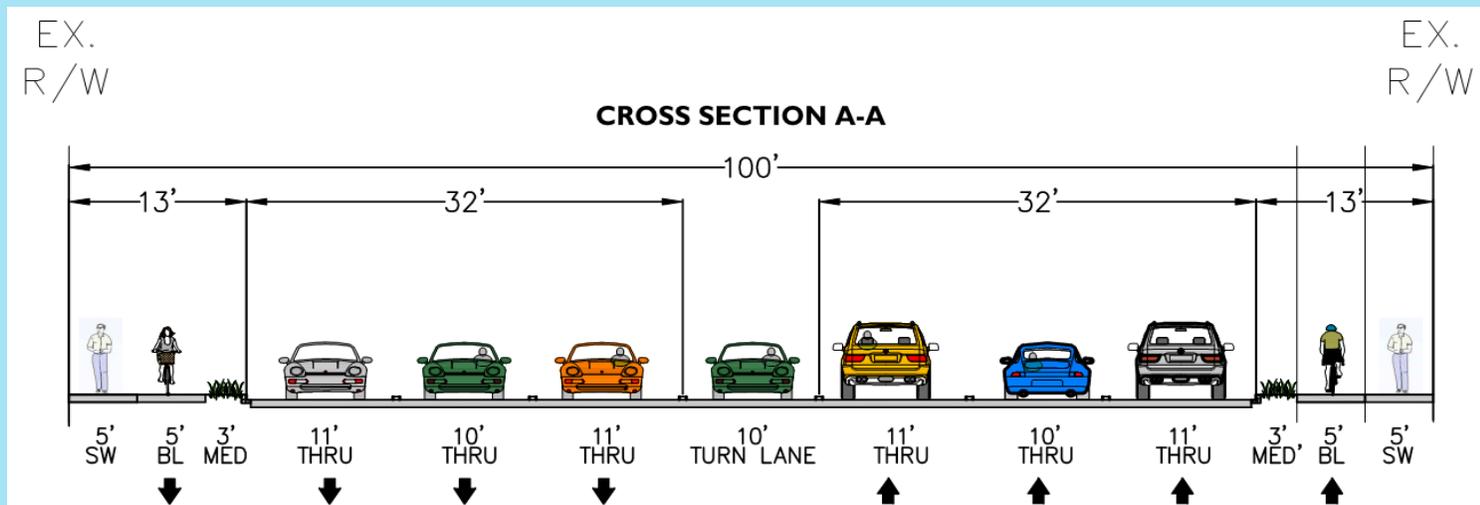


# ALT3 – Typical Cross Section

*At  
Mid-  
Segment*



*At Intersections  
With  
Left-Turn Pocket*



# ALT3 – Street View Rendering



# Project Schedule and Next Steps

- **Conceptual Design and Community Outreach**
  - Bikeway & Walkability Committee Meeting-----Feb. 5, 2020
  - Neighborhood Meeting #1 -----Feb. 26, 2020
  - Conceptual Design Phase -----March-June 2020
  - Neighborhood Meeting #2 -----June 2020
  - City Council Presentation-----Sept. 2020
  - Coordination with SCE for Utility Undergrounding - ongoing
  - Final Design – late 2020



# Questions?

